

JACKSON MUNICIPAL AIRPORT AUTHORITY JACKSON MEDGAR WILEY EVERS INTERNATIONAL AIRPORT JACKSON, MISSISSIPPI

CONSTRUCTION PLANS FOR: TAXIWAY C-6 PAVEMENT REPAIRS

JACKSON MUNICIPAL AIRPORT AUTHORITY STAFF

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DIRECTOR OF FACILITIES
JOYCE TILLMAN



MAY 2022

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Date 2022

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DATE: 5/13/2022



COVER SHEET
REHABILITATE TAXIWAY C6
JACKSON MEDGAR WILEY EVERS INTERNATIONAL AIRPORT
JACKSON MUNICIPAL AIRPORT AUTHORITY
JACKSON, MISSISSIPPI

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SCALE:

PROJECT NO.
J.M.A.A. PROJECT NO.: 013-22

SHEET NO.

1

| SUMMARY OF QUANTITIES – BASE BID | | | | |
|----------------------------------|-------------|---|--------------|------|
| NAICS CODE | ITEM NUMBER | DESCRIPTION | APPROX. QTY. | UNIT |
| 237310 | C-105 | MOBILIZATION AND DEMOBILIZATION | 1 | LS |
| 237310 | P-152-4.1 | UNCLASSIFIED EXCAVATION (ALL DEPTHS)* | 400 | CY |
| 237310 | P-152-4.2 | BORROW EXCAVATION** | 650 | CY |
| 237310 | P-110-4.1A | MILLING OF BITUMINOUS OVERLAY ALL DEPTHS | 100 | SY |
| 237310 | P-401-8.1 | 3" & VAR. BITUMINOUS SUBBASE SURFACE COURSE | 110 | TON |
| 237310 | P-403-8.1 | 8" BITUMINOUS BASE COURSE | 290 | TON |
| 237310 | P-154-5.1 | 6" SUBBASE GRANULAR MATERIAL | 45 | CY |
| 237310 | P-501-8.1 | 11.5" & (VAR.) THICK CONCRETE PANELS | 650 | SY |
| 237310 | P-603-5.1 | EMULSIFIED ASPHALT TACK COAT | 55 | GAL |
| 237310 | P-620-5.1C | TAXIWAY PAINTING, STRIPES(WHITE) | 50 | SF |
| 237310 | P-620-5.1C | TAXIWAY PAINTING, STRIPES(YELLOW) | 750 | LF |
| 237310 | P-620-5.1 | TAXIWAY PAINTING, (RED) | 50 | SF |
| 237310 | P-620-5.1 | TAXIWAY PAINTING, (BLACK) | 815 | SF |
| 238910 | T-901-5.1 | TAXIWAY SEEDING | 1 | ACRE |

NOTE:

* REMOVAL OF ASPHALT & CONCRETE PANELS INCLUDED IN UNCLASSIFIED EXCAVATION

**ITEM TO BE PAID FOR AS DIRECTED BY ENGINEER

| SUMMARY OF QUANTITIES – ALTERNATE BID | | | | |
|---------------------------------------|-------------|---|--------------|------|
| NAICS CODE | ITEM NUMBER | DESCRIPTION | APPROX. QTY. | UNIT |
| 237310 | C-105 | MOBILIZATION AND DEMOBILIZATION | 1 | LS |
| 237310 | P-110-4.1A | MILLING OF BITUMINOUS OVERLAY ALL DEPTHS | 4930 | SY |
| 237310 | P-401-8.1 | 2" & VAR. BITUMINOUS SUBBASE SURFACE COURSE | 550 | TON |
| 237310 | P-603-5.1 | EMULSIFIED ASPHALT TACK COAT | 460 | GAL |
| 237310 | P-620-5.1C | TAXIWAY PAINTING, STRIPES(WHITE) | 480 | SF |
| 237310 | P-620-5.1C | TAXIWAY PAINTING, STRIPES(YELLOW) | 3410 | LF |
| 237310 | P-620-5.1 | TAXIWAY PAINTING, (RED) | 480 | SF |
| 237310 | P-620-5.1 | TAXIWAY PAINTING, (BLACK) | 2450 | SF |

| INDEX TO SHEETS | |
|-----------------|---|
| SHEET NO. | DESCRIPTION |
| 1 | COVER SHEET |
| 2 | SUMMARY OF QUANTITIES AND INDEX TO SHEETS |
| 3 | EXISTING AIRPORT LAYOUT PLAN |
| 4 | PROPOSED AIRPORT LAYOUT PLAN |
| 5 | PROJECT LAYOUT PLAN |
| 6 | PROJECT SAFETY PLAN |
| 7 | PROJECT SAFETY NOTES |
| 8 | C-6 PLAN & PROFILE |
| 9 | CONSTRUCTION SEQUENCING PLAN |
| 10 | MISCELLANEOUS DETAILS |
| 11 | TYPICAL SECTIONS |

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SUMMARY OF QUANTITIES/
INDEX TO SHEETS
REHABILITATE TAXIWAY C6
JACKSON-MEDGAR EVERS INTERNATIONAL AIRPORT
JACKSON MUNICIPAL AIRPORT AUTHORITY
JACKSON, MISSISSIPPI

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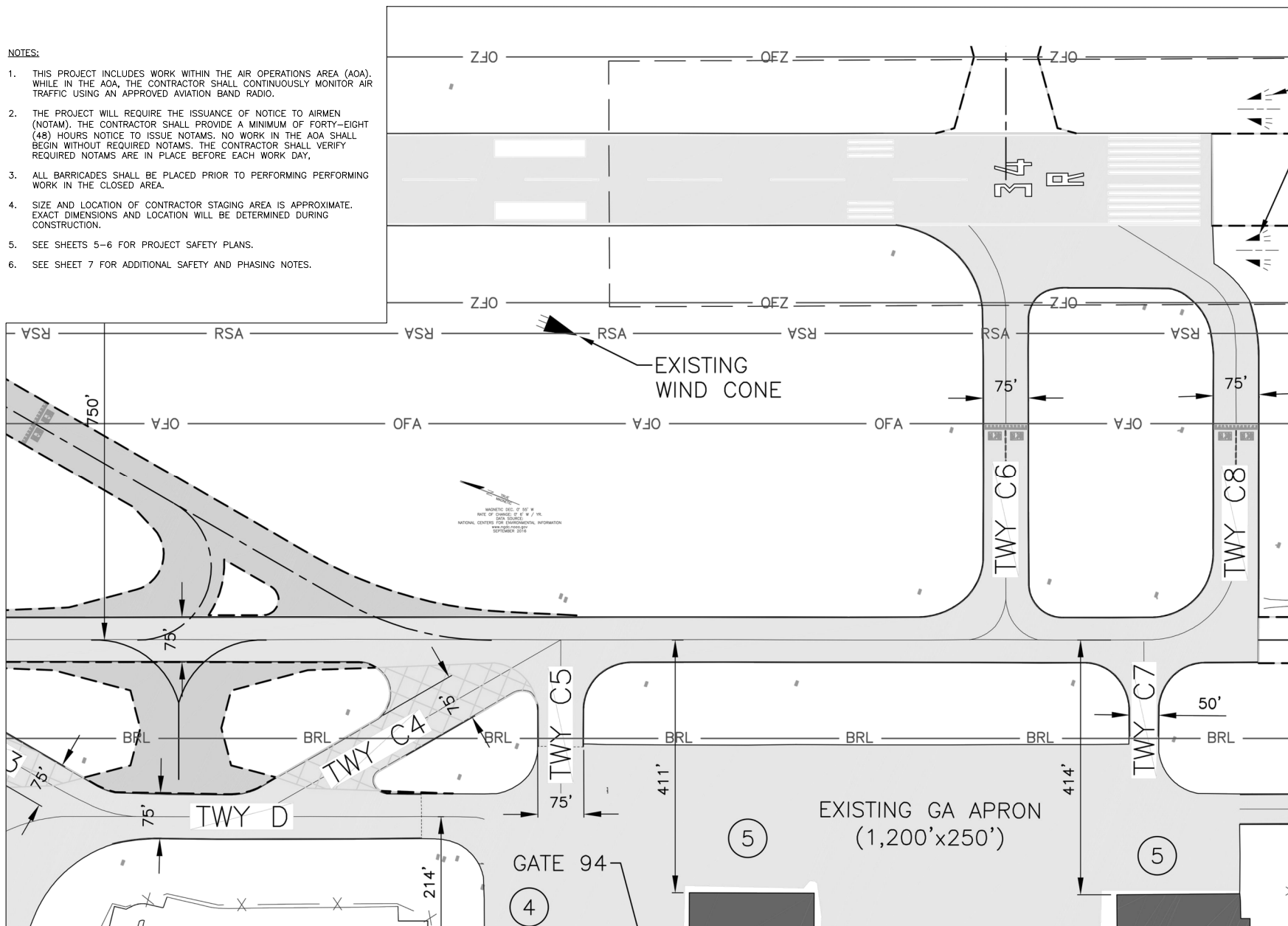
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PROJECT NO.
J.M.A.A. PROJECT NO.: 013-22
A.I.P. NO.

SHEET NO.
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NOTES:

1. THIS PROJECT INCLUDES WORK WITHIN THE AIR OPERATIONS AREA (AOA). WHILE IN THE AOA, THE CONTRACTOR SHALL CONTINUOUSLY MONITOR AIR TRAFFIC USING AN APPROVED AVIATION BAND RADIO.
2. THE PROJECT WILL REQUIRE THE ISSUANCE OF NOTICE TO AIRMEN (NOTAM). THE CONTRACTOR SHALL PROVIDE A MINIMUM OF FORTY-EIGHT (48) HOURS NOTICE TO ISSUE NOTAMS. NO WORK IN THE AOA SHALL BEGIN WITHOUT REQUIRED NOTAMS. THE CONTRACTOR SHALL VERIFY REQUIRED NOTAMS ARE IN PLACE BEFORE EACH WORK DAY.
3. ALL BARRICADES SHALL BE PLACED PRIOR TO PERFORMING WORK IN THE CLOSED AREA.
4. SIZE AND LOCATION OF CONTRACTOR STAGING AREA IS APPROXIMATE. EXACT DIMENSIONS AND LOCATION WILL BE DETERMINED DURING CONSTRUCTION.
5. SEE SHEETS 5-6 FOR PROJECT SAFETY PLANS.
6. SEE SHEET 7 FOR ADDITIONAL SAFETY AND PHASING NOTES.



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PROPOSED AIRPORT LAYOUT PLAN
REHABILITATE TAXIWAY C6
JACKSON-MEDGAR WILEY EVERETT INTERNATIONAL AIRPORT
JACKSON MUNICIPAL AIRPORT AUTHORITY
JACKSON, MISSISSIPPI

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SCALE:

PROJECT NO.
JMAA PROJECT NO: 013-22
A.I.P. NO:

SHEET NO.
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PROJECT LAYOUT PLAN
REHABILITATE TAXIWAY C6
JACKSON-MEDGAR EVERS INTERNATIONAL AIRPORT
JACKSON MUNICIPAL AIRPORT AUTHORITY
JACKSON, MISSISSIPPI

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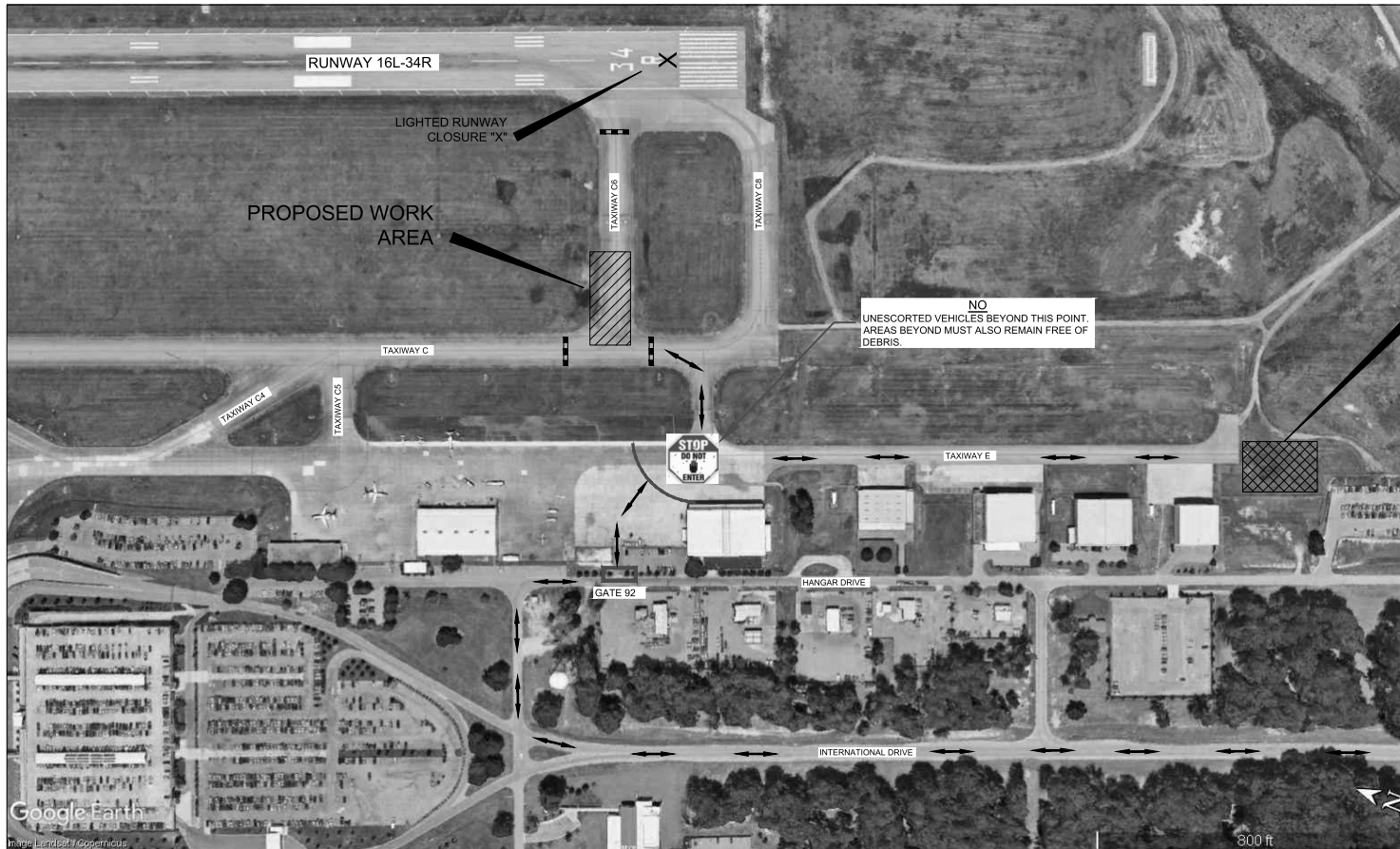
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JMAA PROJECT NO.: 013-22
A.I.P. NO.:

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PROPOSED
LAYDOWN
AREA

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DATE: 5/01/2022



PROJECT SAFETY PLAN
REHABILITATE TAXIWAY C6
JACKSON MEDGAR WILEY EVERS INTERNATIONAL AIRPORT
JACKSON MUNICIPAL AIRPORT AUTHORITY
JACKSON, MISSISSIPPI

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6

SAFETY PLAN NOTES:

1. WORK IN THIS PHASE WILL CONSIST OF THE REQUIRED WORK GENERALLY ALONG TAXIWAY C-6 BETWEEN RUNWAY 16 AND TAXIWAY C. WORK ITEMS INCLUDE THE REMOVAL OF EXISTING FULL DEPTH PAVEMENT AND STRIPING.
2. CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING AND RELOCATING THE SAFETY DEVICES THROUGHOUT THE PROJECT. NO DIRECT PAY.
3. THE CONTRACTOR SHALL MAINTAIN ACCESS THROUGHOUT THE PROJECT SITE SUITABLE FOR AIRCRAFT, EMERGENCY VEHICLES, AND AIRPORT OPERATIONS AS SHOWN ON THE SAFETY PLANS. THE CONTRACTOR SHALL NOT BLOCK SAFETY ACCESS TO OPEN TAXIWAYS AND APRON AREAS.
4. THE CONTRACTOR SHALL ENSURE WORK AREAS AND OPEN TAXIWAYS ARE KEPT CLEAR OF ALL FOD. THE CONTRACTOR SHALL MAINTAIN SUFFICIENT EQUIPMENT ONSITE TO KEEP WORK AREA CLEAN.
5. NO CONSTRUCTION ACTIVITY MAY OCCUR IN THE TAXIWAY SAFETY AREA (TSA) OF AN OPEN TAXIWAY. OPEN TRENCHES OR EXCAVATIONS WITHIN THE TSA WILL BE PERMITTED AS FOLLOWS:
 - A. CURVED SECTIONS OF TAXIWAY – OPEN TRENCHES OR EXCAVATIONS WILL NOT BE PERMITTED WITHIN THE TSA WHILE THE TAXIWAY IS OPEN. TRENCHES AND/OR EXCAVATIONS SHALL BE BACKFILLED OR COVERED BEFORE THE TAXIWAY IS OPENED. COVERING TRENCHES AND/OR EXCAVATIONS SHALL BE SUITABLE TO SUPPORT DESIGN AIRCRAFT.
 - B. STRAIGHT SECTIONS OF TAXIWAY – OPEN TRENCHES AND OR EXCAVATIONS WILL BE PERMITTED WITHIN THE TSA PROVIDED THE AREA MEETS THE MARKING, LIGHTING, AND BARRICADE REQUIREMENTS.

LEGEND:

- APPROXIMATE WORK AREA
- CONTRACTOR'S STAGING AREA
- CONTRACTOR'S HAUL ROUTE
- CONSTRUCTION BARRICADES

CONSTRUCTION SAFETY AND PHASING PLAN (CSPP)

GENERAL:

- THIS CSPP HAS BEEN DEVELOPED IN ACCORDANCE WITH THE APPLICABLE REQUIREMENTS OF FAA ADVISORY 150/5370-2G AND IS CONSIDERED AN INTEGRAL PART TO THE CONTRACTOR'S WORK. THE FOLLOWING DRAWINGS ARE CONSIDERED PART OF THE CSPP:
 - SHEET 2 SUMMARY OF QUANTITIES
 - SHEET 4 PROPOSED PROJECT LAYOUT PLAN
 - SHEET 5 PROJECT LAYOUT PLAN
 - SHEET 6 PROJECT SAFETY PLAN
 - SHEET 7 PROJECT SAFETY NOTES
- A FAA 7460 HAS BEEN SUBMITTED TO THE FAA FOR REVIEW. THIS SUBMITTAL WAS BASED ON A MAXIMUM HEIGHT OF 20 FT. THE DETERMINATION OF THIS SUBMITTAL HAS BEEN INCORPORATED IN THE CSPP. THE CONTRACTOR SHALL INCLUDE THE MAXIMUM EQUIPMENT HEIGHT IN THE PROPOSAL ON CORPORATE LETTERHEAD.
- THE CONTRACTOR SHALL SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) FOR REVIEW AND APPROVAL. FOR INFORMATION PERTAINING TO THE REQUIREMENTS OF THIS DOCUMENT, REFER TO THE SECTION "CONTRACTOR RESPONSIBILITIES".
- ALL PARTIES DRIVING IN THE AOA WILL COMPLETE THE AIRPORTS DRIVER'S TRAINING PRIOR TO THE PROJECT START. PENALTIES FOR AIRFIELD INCURSIONS INCLUDE BUT NOT LIMITED TO LOSS OF AOA ACCESS AND FINES UP TO \$10,000 PER INCIDENT PER PERSON.
- CONTACT INFORMATION:
 - AIRPORT OWNER: PAUL A. BROWN, CEO
(601) 664-3504
 - AIRPORT TOWER: NAME
NUMBER
 - AIRPORT OPERATIONS STAFF: AUSTIN JENKINS
(601) 850-0252
 - ENGINEER: EJES
GREGORY KORB, PE
(601) 691-1060
 - CONTRACTOR: T.B.D

CONTRACTOR RESPONSIBILITIES:

- SUBMIT A SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) TO THE AIRPORT OPERATOR DESCRIBING HOW IT WILL COMPLY WITH THE REQUIREMENTS OF THE CSPP AND SUPPLYING ANY DETAILS THAT COULD NOT BE DETERMINED BEFORE CONTRACT AWARD. THE SPCD MUST INCLUDE A CERTIFICATION STATEMENT BY THE CONTRACTOR THAT INDICATES IT UNDERSTANDS THE OPERATIONAL SAFETY REQUIREMENTS OF THE CSPP AND IT ASSERTS IT WILL NOT DEVIATE FROM THE APPROVED CSPP AND SPCD UNLESS WRITTEN APPROVAL IS GRANTED BY THE AIRPORT OPERATOR. ANY CONSTRUCTION PRACTICE PROPOSED BY THE CONTRACTOR THAT DOES NOT CONFORM TO THE CSPP AND SPCD MAY IMPACT THE AIRPORT'S OPERATIONAL SAFETY AND WILL REQUIRE A REVISION TO THE CSPP AND SPCD AND RE-COORDINATION WITH THE AIRPORT OPERATOR AND THE FAA IN ADVANCE.
- HAVE AVAILABLE AT ALL TIMES COPIES OF THE CSPP AND SPCD FOR REFERENCE BY THE AIRPORT OPERATOR AND ITS REPRESENTATIVES, AND BY SUBCONTRACTORS AND CONTRACTOR EMPLOYEES.
- ENSURE THAT CONSTRUCTION PERSONNEL ARE FAMILIAR WITH SAFETY PROCEDURES AND REGULATIONS ON THE AIRPORT. PROVIDE A POINT OF CONTACT WHO WILL COORDINATE AN IMMEDIATE RESPONSE TO CORRECT ANY CONSTRUCTION-RELATED ACTIVITY THAT MAY ADVERSELY AFFECT THE OPERATIONAL SAFETY OF THE AIRPORT. MANY PROJECTS WILL REQUIRE 24-HOUR COVERAGE.
- IDENTIFY IN THE SPCD THE CONTRACTOR'S ON-SITE EMPLOYEES RESPONSIBLE FOR MONITORING COMPLIANCE WITH THE CSPP AND SPCD DURING CONSTRUCTION. AT LEAST ONE OF THESE EMPLOYEES MUST BE ON-SITE WHENEVER ACTIVE CONSTRUCTION IS TAKING PLACE.
- CONDUCT INSPECTIONS WITH SUFFICIENT FREQUENCY TO ENSURE CONSTRUCTION PERSONNEL COMPLY WITH THE CSPP AND SPCD AND THAT THERE ARE NO ALTERED CONSTRUCTION ACTIVITIES THAT COULD CREATE POTENTIAL SAFETY HAZARDS
- RESTRICT MOVEMENT OF CONSTRUCTION VEHICLES AND PERSONNEL TO PERMITTED CONSTRUCTION AREAS BY FLAGGING, BARRICADING, ERECTING TEMPORARY FENCING, OR PROVIDING ESCORTS, AS APPROPRIATE AND AS SPECIFIED IN THE CSPP AND SPCD. THE CONTRACTOR SHALL NOT DIRECT AIRCRAFT.
- ENSURE THAT NO CONTRACTOR EMPLOYEES, EMPLOYEES OF SUBCONTRACTORS OR SUPPLIERS, OR OTHER PERSONS ENTER ANY PART OF THE AIR OPERATIONS AREA (AOA) FROM THE CONSTRUCTION SITE UNLESS AUTHORIZED.
- PROVIDE ADEQUATE LIGHT UNITS FOR ALL NIGHT WORK. THE LOCATION AND DIRECTION OF LIGHT UNITS SHALL BE COORDINATED WITH THE OWNER AND FAA ATCT TO ENSURE NO DISRUPTION TO AIR TRAFFIC AND ATCT VISIBILITY.
- TAKE NECESSARY PRECAUTIONS TO CONTROL F.O.D. ie TRASH, DEBRIS, ETC IN THE WORK AREAS. IF ANY DEBRIS LEAVES THE WORK AREA IT SHALL BE RETRIEVED IMMEDIATELY.
- TAKE THE NECESSARY PRECAUTIONS TO PROTECT THE WORK AREA FROM JET BLAST. ITEMS NOT BEING ACTIVELY USED IN WORK EFFORT SHALL BE PROPERLY STORED OR SECURED.
- THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CONTROL OF DUST ON THE PROJECT AND SHALL MAINTAIN SUFFICIENT EQUIPMENT AND MANPOWER TO MITIGATE DUST AS REQUIRED.
- COORDINATION. DISCUSS DETAILS OF PROPOSED SAFETY MEETINGS WITH THE AIRPORT OPERATOR AND WITH CONTRACTOR EMPLOYEES AND SUBCONTRACTORS.
- PHASING. DISCUSS PROPOSED CONSTRUCTION SCHEDULE ELEMENTS, INCLUDING:
 - DURATION OF EACH PHASE.
 - DAILY START AND FINISH OF CONSTRUCTION, INCLUDING "NIGHT ONLY" CONSTRUCTION.
 - DURATION OF CONSTRUCTION ACTIVITIES DURING NORMAL RUNWAY OPERATIONS, CLOSED RUNWAY OPERATIONS AND MODIFIED RUNWAY "AIRCRAFT REFERENCE CODE" USAGE.
- AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY. THESE AREAS ARE SHOWN ON THE CSPP.
- PROTECTION OF NAVAIDS. DISCUSS SPECIFIC METHODS PROPOSED TO PROTECT OPERATING NAVAIDS.
- CONTRACTOR ACCESS. PROVIDE THE FOLLOWING:
 - DETAILS ON HOW THE CONTRACTOR WILL MAINTAIN THE INTEGRITY OF THE AIRPORT SECURITY FENCE (GATE GUARDS, DAILY LOG OF CONSTRUCTION PERSONNEL, AND OTHER).
 - LISTING OF INDIVIDUALS REQUIRING DRIVER TRAINING (FOR CERTIFICATED AIRPORTS AND AS REQUESTED).
 - RADIO COMMUNICATIONS INCLUDING: TYPES OF RADIOS AND BACKUP CAPABILITIES; WHO WILL BE MONITORING RADIOS; WHOM TO CONTACT IF THE ATCT CANNOT REACH THE CONTRACTOR'S DESIGNATED PERSON BY RADIO.
 - DETAILS ON HOW THE CONTRACTOR WILL ESCORT MATERIAL DELIVERY VEHICLES.
- WILDLIFE MANAGEMENT. DISCUSS THE FOLLOWING:
 - METHODS AND PROCEDURES TO PREVENT WILDLIFE ATTRACTION.
 - WILDLIFE REPORTING PROCEDURES.
- FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT. DISCUSS EQUIPMENT AND METHODS FOR CONTROL OF FOD, INCLUDING CONSTRUCTION DEBRIS AND DUST.
- HAZARDOUS MATERIAL (HAZMAT) MANAGEMENT. DISCUSS EQUIPMENT AND METHODS FOR RESPONDING TO HAZARDOUS SPILLS.
- NOTIFICATION OF CONSTRUCTION ACTIVITIES. PROVIDE THE FOLLOWING:
 - CONTRACTOR'S POINTS OF CONTACT.
 - CONTRACTOR'S EMERGENCY CONTACT.
 - LISTING OF TALL OR OTHER REQUESTED EQUIPMENT PROPOSED FOR USE ON THE AIRPORT AND THE TIMEFRAME FOR SUBMITTING 7460-1 FORMS NOT PREVIOUSLY SUBMITTED BY THE AIRPORT OPERATOR.
 - BATCH PLANT DETAILS, INCLUDING 7460-1 SUBMITTAL.
- INSPECTION REQUIREMENTS. DISCUSS DAILY (OR MORE FREQUENT) INSPECTIONS AND SPECIAL INSPECTION PROCEDURES.
- UNDERGROUND UTILITIES. DISCUSS PROPOSED METHODS OF IDENTIFYING AND PROTECTING UNDERGROUND UTILITIES.
- PENALTIES. PENALTIES SHOULD BE IDENTIFIED IN THE CSPP AND SHOULD NOT REQUIRE AN ENTRY IN THE SPCD.
- SPECIAL CONDITIONS. DISCUSS PROPOSED ACTIONS FOR EACH SPECIAL CONDITION IDENTIFIED IN THE CSPP.
- RUNWAY AND TAXIWAY VISUAL AIDS. INCLUDING MARKING, LIGHTING, SIGNS, AND VISUAL NAVAIDS. DISCUSS PROPOSED VISUAL AIDS INCLUDING THE FOLLOWING:
 - EQUIPMENT AND METHODS FOR COVERING SIGNAGE AND AIRFIELD LIGHTS.
 - EQUIPMENT AND METHODS FOR TEMPORARY CLOSURE MARKINGS (PAINT, FABRIC, OTHER).
 - TYPES OF TEMPORARY VISUAL GUIDANCE SLOPE INDICATORS (VGS).
- MARKING AND SIGNS FOR ACCESS ROUTES. DISCUSS PROPOSED METHODS OF DEMARCATING ACCESS ROUTES FOR VEHICLE DRIVERS. ALL ROADS USED SHALL BE INSPECTED TWICE DAILY FOR CONSTRUCTION DEBRIS. ANY DAMAGE DONE TO EXISTING ROADS, SIGNS, ETC. SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.

THE CONTRACTOR'S SAFETY PLAN COMPLIANCE DOCUMENT SHALL CONFORM TO THE FOLLOWING FORMAT AND MINIMUM REQUIREMENTS:

- COORDINATION. DISCUSS DETAILS OF PROPOSED SAFETY MEETINGS WITH THE AIRPORT OPERATOR AND WITH CONTRACTOR EMPLOYEES AND SUBCONTRACTORS.
- PHASING. DISCUSS PROPOSED CONSTRUCTION SCHEDULE ELEMENTS, INCLUDING:
 - DURATION OF EACH PHASE.
 - DAILY START AND FINISH OF CONSTRUCTION, INCLUDING "NIGHT ONLY" CONSTRUCTION.
 - DURATION OF CONSTRUCTION ACTIVITIES DURING NORMAL RUNWAY OPERATIONS, CLOSED RUNWAY OPERATIONS AND MODIFIED RUNWAY "AIRCRAFT REFERENCE CODE" USAGE.
- AREAS AND OPERATIONS AFFECTED BY THE CONSTRUCTION ACTIVITY. THESE AREAS ARE SHOWN ON THE CSPP.
- PROTECTION OF NAVAIDS. DISCUSS SPECIFIC METHODS PROPOSED TO PROTECT OPERATING NAVAIDS.
- CONTRACTOR ACCESS. PROVIDE THE FOLLOWING:
 - DETAILS ON HOW THE CONTRACTOR WILL MAINTAIN THE INTEGRITY OF THE AIRPORT SECURITY FENCE (GATE GUARDS, DAILY LOG OF CONSTRUCTION PERSONNEL, AND OTHER).
 - LISTING OF INDIVIDUALS REQUIRING DRIVER TRAINING (FOR CERTIFICATED AIRPORTS AND AS REQUESTED).
 - RADIO COMMUNICATIONS INCLUDING: TYPES OF RADIOS AND BACKUP CAPABILITIES; WHO WILL BE MONITORING RADIOS; WHOM TO CONTACT IF THE ATCT CANNOT REACH THE CONTRACTOR'S DESIGNATED PERSON BY RADIO.
 - DETAILS ON HOW THE CONTRACTOR WILL ESCORT MATERIAL DELIVERY VEHICLES.
- WILDLIFE MANAGEMENT. DISCUSS THE FOLLOWING:
 - METHODS AND PROCEDURES TO PREVENT WILDLIFE ATTRACTION.
 - WILDLIFE REPORTING PROCEDURES.
- FOREIGN OBJECT DEBRIS (FOD) MANAGEMENT. DISCUSS EQUIPMENT AND METHODS FOR CONTROL OF FOD, INCLUDING CONSTRUCTION DEBRIS AND DUST.
- HAZARDOUS MATERIAL (HAZMAT) MANAGEMENT. DISCUSS EQUIPMENT AND METHODS FOR RESPONDING TO HAZARDOUS SPILLS.
- NOTIFICATION OF CONSTRUCTION ACTIVITIES. PROVIDE THE FOLLOWING:
 - CONTRACTOR'S POINTS OF CONTACT.
 - CONTRACTOR'S EMERGENCY CONTACT.
 - LISTING OF TALL OR OTHER REQUESTED EQUIPMENT PROPOSED FOR USE ON THE AIRPORT AND THE TIMEFRAME FOR SUBMITTING 7460-1 FORMS NOT PREVIOUSLY SUBMITTED BY THE AIRPORT OPERATOR.
 - BATCH PLANT DETAILS, INCLUDING 7460-1 SUBMITTAL.
- INSPECTION REQUIREMENTS. DISCUSS DAILY (OR MORE FREQUENT) INSPECTIONS AND SPECIAL INSPECTION PROCEDURES.
- UNDERGROUND UTILITIES. DISCUSS PROPOSED METHODS OF IDENTIFYING AND PROTECTING UNDERGROUND UTILITIES.
- PENALTIES. PENALTIES SHOULD BE IDENTIFIED IN THE CSPP AND SHOULD NOT REQUIRE AN ENTRY IN THE SPCD.
- SPECIAL CONDITIONS. DISCUSS PROPOSED ACTIONS FOR EACH SPECIAL CONDITION IDENTIFIED IN THE CSPP.
- RUNWAY AND TAXIWAY VISUAL AIDS. INCLUDING MARKING, LIGHTING, SIGNS, AND VISUAL NAVAIDS. DISCUSS PROPOSED VISUAL AIDS INCLUDING THE FOLLOWING:
 - EQUIPMENT AND METHODS FOR COVERING SIGNAGE AND AIRFIELD LIGHTS.
 - EQUIPMENT AND METHODS FOR TEMPORARY CLOSURE MARKINGS (PAINT, FABRIC, OTHER).
 - TYPES OF TEMPORARY VISUAL GUIDANCE SLOPE INDICATORS (VGS).
- MARKING AND SIGNS FOR ACCESS ROUTES. DISCUSS PROPOSED METHODS OF DEMARCATING ACCESS ROUTES FOR VEHICLE DRIVERS. ALL ROADS USED SHALL BE INSPECTED TWICE DAILY FOR CONSTRUCTION DEBRIS. ANY DAMAGE DONE TO EXISTING ROADS, SIGNS, ETC. SHALL BE REPAIRED OR REPLACED BY THE CONTRACTOR AT NO ADDITIONAL COST TO THE OWNER.

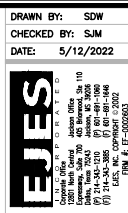
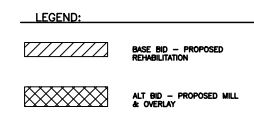
- HAZARD MARKING AND LIGHTING. DISCUSS PROPOSED EQUIPMENT AND METHODS FOR IDENTIFYING EXCAVATION AREAS.
- PROTECTION OF RUNWAY AND TAXIWAY SAFETY AREAS INCLUDING OBJECT FREE AREAS, OBSTACLE FREE ZONES, AND APPROACH/DEPARTURE SURFACES. DISCUSS PROPOSED METHODS OF IDENTIFYING, DEMARCATING, AND PROTECTING AIRPORT SURFACES INCLUDING:
 - EQUIPMENT AND METHODS FOR MAINTAINING TAXIWAY SAFETY AREA STANDARDS.
 - EQUIPMENT AND METHODS FOR SEPARATION OF CONSTRUCTION OPERATIONS FROM AIRCRAFT OPERATIONS, INCLUDING DETAILS OF BARRICADES.
- OTHER LIMITATIONS ON CONSTRUCTION SHOULD BE IDENTIFIED IN THE CSPP AND SHOULD NOT REQUIRE AN ENTRY IN THE SPCD. THE CONTRACTOR'S SAFETY PLAN COMPLIANCE DOCUMENT (SPCD) SHALL INCLUDE A STATEMENT BY THE CONSTRUCTION CONTRACTOR THAT HE/SHE HAS READ AND WILL ABIDE BY THE CSPP. IN ADDITION, THE SPCD MUST INCLUDE ALL SUPPLEMENTAL INFORMATION THAT COULD NOT BE INCLUDED IN THE CSPP PRIOR TO THE CONTRACT AWARD. THE CONTRACTOR STATEMENT SHOULD INCLUDE THE NAME OF THE CONTRACTOR, THE TITLE OF THE PROJECT CSPP, THE APPROVAL DATE OF THE CSPP, AND A REFERENCE TO ANY SUPPLEMENTAL INFORMATION (THAT IS, "1. NAME OF CONTRACTOR, HAVE READ THE TITLE OF PROJECT CSPP, APPROVED ON DATE, AND WILL ABIDE BY IT AS WRITTEN AND WITH THE FOLLOWING ADDITIONS AS NOTED."); THE). THE SUPPLEMENTAL INFORMATION IN THE SPCD SHOULD BE WRITTEN TO MATCH THE FORMAT OF THE CSPP INDICATING EACH SUBJECT BY CORRESPONDING CSPP SUBJECT NUMBER AND TITLE. IF NO SUPPLEMENTAL INFORMATION IS NECESSARY FOR ANY SPECIFIC SUBJECT, THE STATEMENT, "NO SUPPLEMENTAL INFORMATION," NO SUPPLEMENTAL INFORMATION," SHOULD BE WRITTEN AFTER THE CORRESPONDING SUBJECT TITLE. THE SPCD SHOULD NOT DUPLICATE INFORMATION IN THE CSPP.

THE AIRPORT OPERATOR WILL:

- DEVELOP A CSPP THAT COMPLIES WITH THE SAFETY GUIDELINES OF FAA ADVISORY CIRCULAR 150/5370-2G.
- REQUIRE THE REVIEW AND APPROVAL OF THE SPCD PROVIDED BY THE CONTRACTOR. THIS SPCD SHALL INDICATE HOW THE CONTRACTOR WILL COMPLY WITH THE CSPP AND PROVIDE DETAILS THAT CANNOT BE DETERMINED BEFORE CONTRACT AWARD.
- CONVENE A PRECONSTRUCTION MEETING WITH THE CONSTRUCTION CONTRACTOR, ENGINEER, AIRPORT EMPLOYEES AND TENANT REPRESENTATIVES (IF APPROPRIATE) TO REVIEW AND DISCUSS PROJECT SAFETY BEFORE BEGINNING CONSTRUCTION ACTIVITY. THE FAA WILL BE INVITED TO ATTEND THIS MEETING.
- ENSURE CONTACT INFORMATION IS ACCURATE FOR EACH REPRESENTATIVE/POINT OF CONTACT IDENTIFIED IN THE CSPP.
- HOLD WEEKLY OR, IF NECESSARY, DAILY SAFETY MEETINGS WITH ALL AFFECTED PARTIES TO COORDINATE ACTIVITIES.
- NOTIFY USERS, ARFF PERSONNEL, AND FAA ATO PERSONNEL OF CONSTRUCTION AND CONDITIONS THAT MAY ADVERSELY AFFECT THE OPERATIONAL SAFETY OF THE AIRPORT VIA NOTICES TO AIRMEN (NOTAM) AND OTHER METHODS, AS APPROPRIATE. CONVENE A MEETING FOR REVIEW AND DISCUSSION IF NECESSARY.
- ENSURE CONSTRUCTION PERSONNEL KNOW OF ANY APPLICABLE AIRPORT PROCEDURES AND OF CHANGES TO THOSE PROCEDURES THAT MAY AFFECT THEIR WORK.
- ENSURE CONSTRUCTION CONTRACTORS AND SUBCONTRACTORS UNDERGO TRAINING REQUIRED BY THE CSPP AND SPCD.
- ENSURE VEHICLE AND PEDESTRIAN OPERATIONS ADDRESSED IN THE CSPP AND SPCD ARE COORDINATED WITH AIRPORT TENANTS, THE AIRPORT TRAFFIC CONTROL TOWER (ATCT), AND CONSTRUCTION CONTRACTORS.
- AT CERTIFICATED AIRPORTS, ENSURE EACH CSPP AND SPCD IS CONSISTENT WITH PART 139.
- CONDUCT INSPECTIONS SUFFICIENTLY FREQUENTLY TO ENSURE CONSTRUCTION CONTRACTORS AND TENANTS COMPLY WITH THE CSPP AND SPCD AND THAT THERE ARE NO ALTERED CONSTRUCTION ACTIVITIES THAT COULD CREATE POTENTIAL SAFETY HAZARDS.
- RESOLVE SAFETY DEFICIENCIES IMMEDIATELY. AT AIRPORTS SUBJECT TO 49 CFR PART 1542, AIRPORT SECURITY, ENSURE CONSTRUCTION ACCESS COMPLIES WITH THE SECURITY REQUIREMENTS OF THAT REGULATION.
- NOTIFY APPROPRIATE PARTIES WHEN CONDITIONS EXIST THAT INVOKE PROVISIONS OF THE CSPP AND SPCD (FOR EXAMPLE, IMPLEMENTATION OF LOW-VISIBILITY OPERATIONS).
- ENSURE PROMPT SUBMITTAL OF A NOTICE OF PROPOSED CONSTRUCTION OR ALTERATION (FORM 7460-1) FOR CONDUCTING AN AERONAUTICAL STUDY OF POTENTIAL OBSTRUCTIONS SUCH AS TALL EQUIPMENT (CRANES, CONCRETE PUMPS, ETC.), STOCK PILES, AND HAUL ROUTES. A SEPARATE FORM MAY BE FILED FOR EACH POTENTIAL OBSTRUCTION, OR ONE FORM MAY BE FILED DESCRIBING THE ENTIRE CONSTRUCTION AREA AND MAXIMUM EQUIPMENT HEIGHT. IN THE LATTER CASE, A SEPARATE FORM MUST BE FILED FOR ANY OBJECT BEYOND OR HIGHER THAN THE ORIGINALLY EVALUATED AREA/HEIGHT.

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CHECKED BY: SJM
DATE: 5/01/2022





C-6 PLAN & PROFILE

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PROJECT NO.
J.M.A.A. PROJECT NO.: 013-22
A.I.P. NO.:

SHEET NO.

8




CONSTRUCTION SEQUENCING NOTES PHASE I:

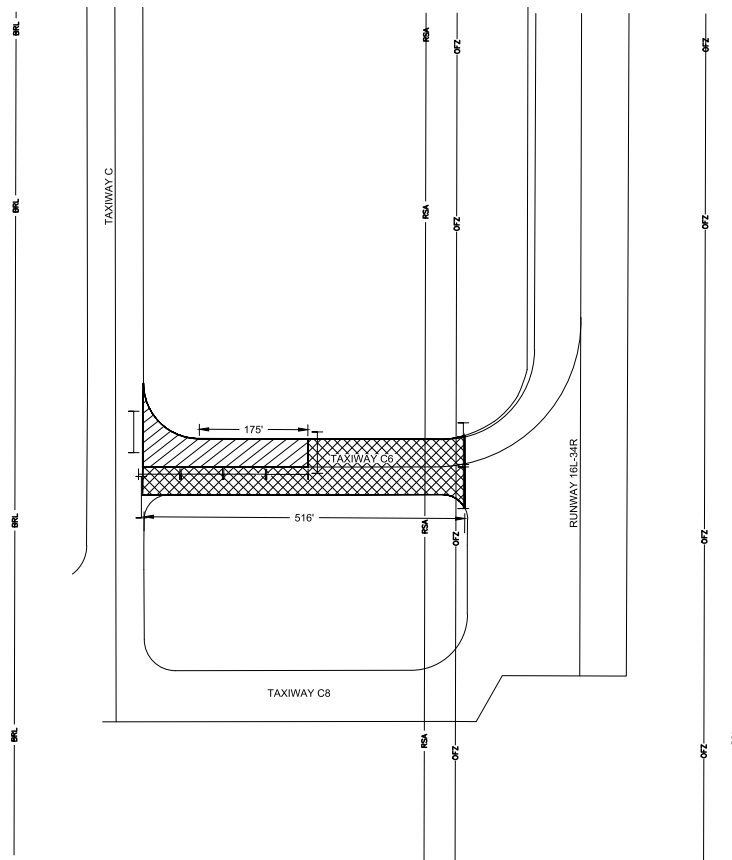
1. PHASE I INCLUDES CONTRACTOR MOBILIZATION, SETTING UP ACCESS AND STAGING AREA, STOCKPILING MATERIALS AND JOB MIX FORMULA SUBMITTALS.
2. PHASE I INCLUDES THE REMOVAL OF EXISTING ASPHALT PAVEMENT SECTIONS AS NOTED IN TAXIWAY C6..
3. PHASE I SHALL BE COMPLETED WITHIN 90 CALENDAR DAYS AFTER THE NOTICE TO PROCEED IS ISSUED TO THE CONTRACTOR..
4. THE CONTRACTOR SHALL NOTIFY THE ENGINEER AND AIRPORT MANAGER 7 DAYS IN ADVANCE OF HIS INTENT TO BEGIN THE WORK SO THAT THE AIRPORT MAY ISSUE THE APPROPRIATE NOTIFICATIONS..
5. AT THE CONCLUSION OF PHASE I, THE WORK SHALL BE INSPECTED BY THE CONTRACTOR, THE ENGINEER, AND THE AIRPORT'S REPRESENTATIVE. WORK SHALL NOTE COMMENCE ON PHASE II UNTIL SUCH TIME AS PHASE I CONSTRUCTION IS COMPLETE AND ACCEPTED.

GENERAL NOTES :

1. ALL PERSONNEL AND EQUIPMENT SHALL REMAIN CLEAR OF THE RUNWAY. THE SOUTH APRON SHALL REMAIN OPERATIONAL DURING EACH INDIVIDUAL PHASE..
2. THE CONTRACTOR SHALL UTILIZE THE STAGING AREA AND FOLLOW THE HAUL ROUTE IDENTIFIED IN SHEET 6.
3. IN THE EVENT THAT OTHER CONSTRUCTION PROJECTS ARE IN PROGRESS AT THE TIME OF THIS CONTRACT, THE CONTRACTOR IS REQUIRED TO COOPERATE WITH ALL OTHER CONTRACTORS AN AIRPORT MANAGEMENT IN THE COORDINATION OF THE WORK.
4. AIRPORT MANAGEMENT WILL AT ALL TIMES HAVE JURISDICTION OVER THE SAFETY OF AIR OR GROUND TRAFFIC DURING CONSTRUCTION. WHENEVER THE SAFETY OF AIR OR GROUND TRAFFIC DURING CONSTRUCTION IS CONCERNED, THEIR DECISIONS TO METHODS, PROCEDURES AND MEASURES USED SHALL BE FINAL, AND ANY AND ALL CONTRACTORS PERFORMING WORK MUST BE GOVERNED BY SUCH DECISIONS.
5. THE CONTRACTOR SHALL NOT BE ENTITLED TO COMPENSATION DUE TO DELAYS OR INCONVENIENCES CAUSED BY SAID NECESSARY METHODS, PROCEDURES AND MEASURES TO ACCOMMODATE AIR AND GROUND TRAFFIC.

LEGEND:

-  PROPOSED REHABILITATION (BASE BID ITEM)
-  MODIFIED TYPE II BARRICADES EVENLY SPACED (10' MAX)
-  PROPOSED MILL & OVERLAY (ALT BID ITEM)



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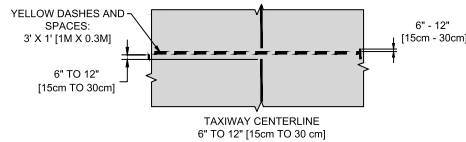
CONSTRUCTION SEQUENCE PLAN
REHABILITATE TAXIWAY C6
JACKSON-MEDGAR WILEY EVERETT INTERNATIONAL AIRPORT
JACKSON MUNICIPAL AIRPORT AUTHORITY
JACKSON, MISSISSIPPI

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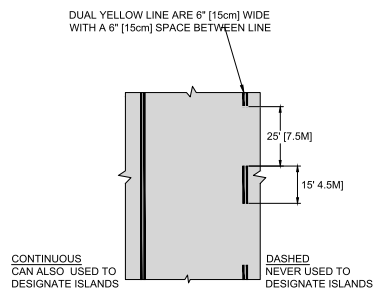
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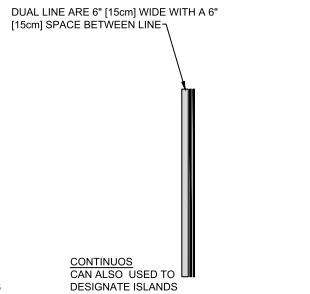
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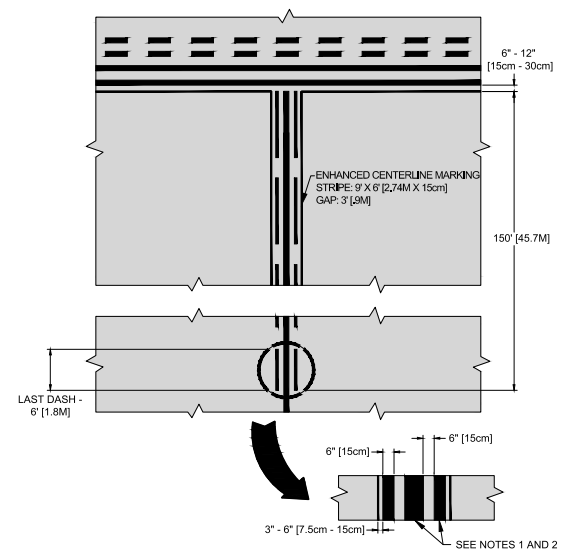
PATTERN C - INTERMEDIATE HOLDING POSITION MARKING FOR TAXIWAY/TAXIWAY INTERSECTIONS
NO SCALE



TAXIWAY EDGE MARKINGS - 2 CASES
NO SCALE



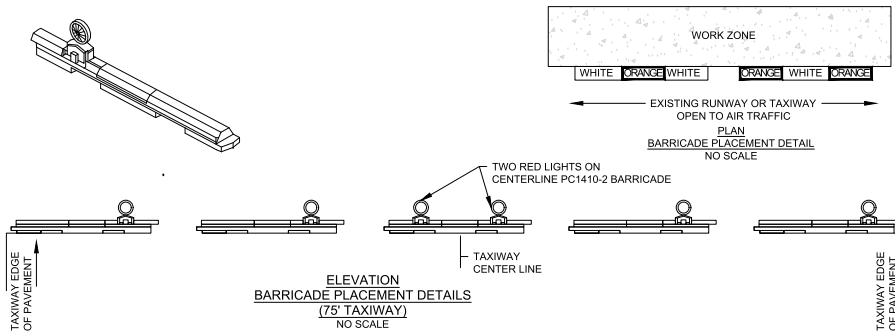
ENHANCED TAXIWAY EDGE MARKINGS
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ENHANCED CENTERLINE MARKING DETAIL
NO SCALE

NOTE:

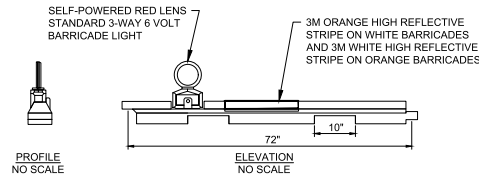
1. DASHED LINE FOR THE ENHANCED TAXIWAY CENTERLINE MARKING ARE 6" (15cm) IN WIDTH AND SEPARATED 6" (15cm) FROM THE TAXIWAY CENTERLINE. THIS APPLIES TO BOTH 6" (15cm) AND 12" (30cm) TAXIWAY CENTERLINE MARKINGS.
2. THE TAXIWAY CENTERLINE MARKINGS MAY BE SHIFTED LEFT OR RIGHT TO AVOID INTERFERENCE WITH THE TAXIWAY CENTERLINE LIGHTS.



ELEVATION BARRICADE PLACEMENT DETAILS (75' TAXIWAY)
NO SCALE

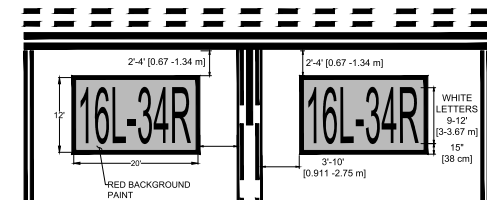
NOTE:

1. BARRICADE CONSTRUCTED OF HIGH IMPACT, UV-RESISTANT POLYETHYLENE AS MANUFACTURED BY NEUBERT AERO CORP. (1-877-NAC-ARPT) OR SIMILAR APPROVED EQUAL.
2. EACH UNIT SHALL INCLUDE RED STANDARD 3-WAY LIGHT, 12" ORANGE FLAG AND 3M HIGH REFLECTIVE ORANGE TAPE.
3. BARRICADES SHALL BE PLACED AS SHOWN UNLESS OTHERWISE DIRECTED BY THE RESIDENT ENGINEER.
4. BARRICADES ARE TO BE PROPERTY OF THE AIRPORT UPON COMPLETION OF THE PROJECT.



PROFILE
NO SCALE

ELEVATION
NO SCALE



THE INSCRIPTION MUST HAVE A HEIGHT OF 12" (3.05 m); HOWEVER, IT MAY BE REDUCED, AS NECESSARY, TO THE MINIMUM HEIGHT OF 9" (2.29 m)

TAXIWAY HOLDING LINE MARKINGS
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DATE: 5/01/2022



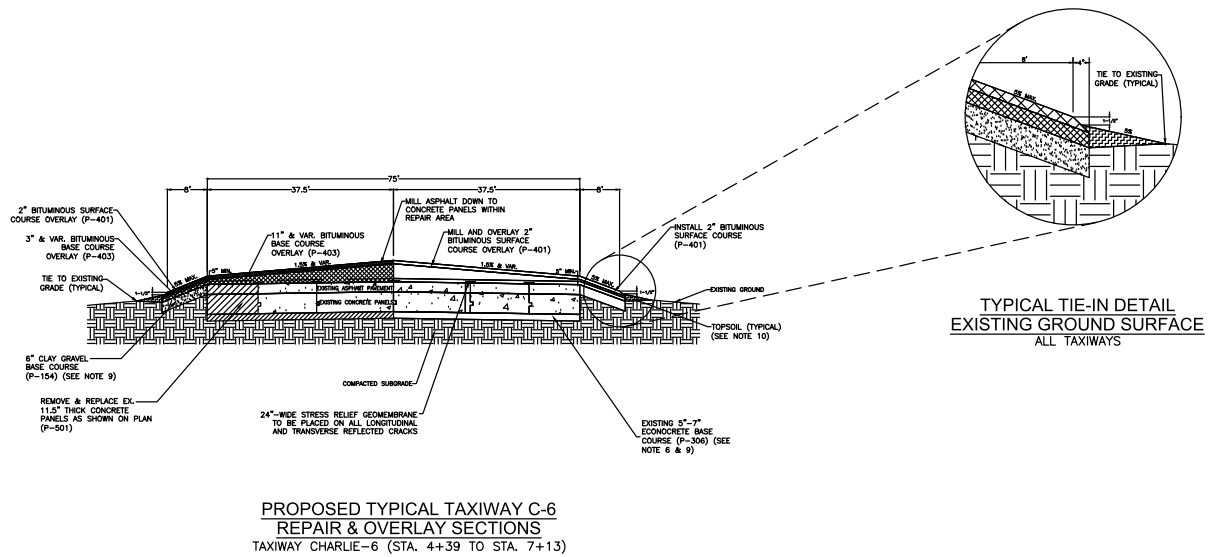
MISCELLANEOUS DETAILS
REHABILITATE TAXIWAY C6
JACKSON-MEDGAR WILEY EVERETT INTERNATIONAL AIRPORT
JACKSON MUNICIPAL AIRPORT AUTHORITY
JACKSON, MISSISSIPPI

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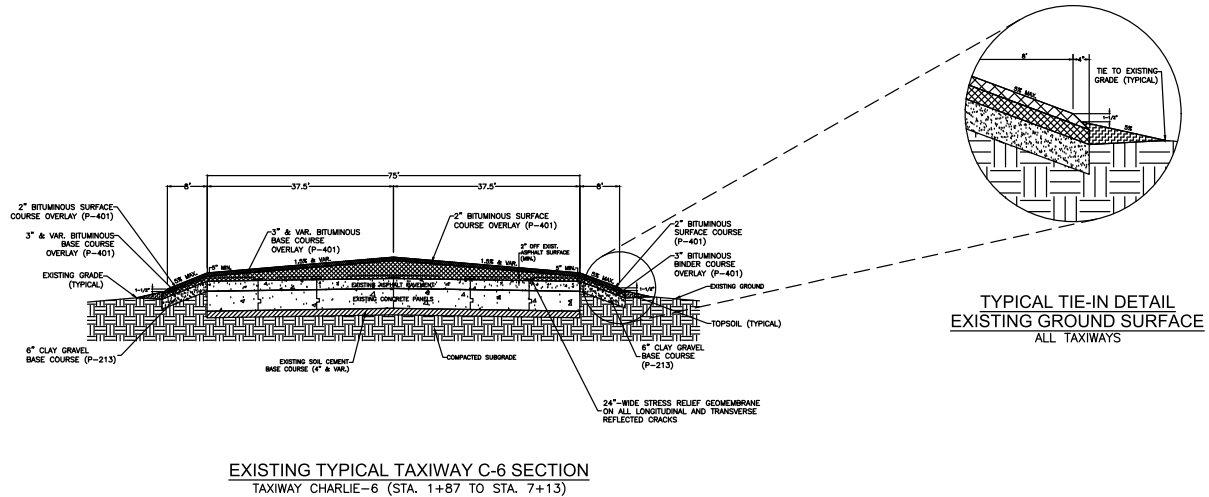
PROJECT NO.
JMAA PROJECT NO.: 013-22
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SHEET NO.
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NOTES:

1. TAXIWAY CHARLIE (C) AND ALL OTHER CONNECTOR TAXIWAYS ARE 75 FT. IN WIDTH.
2. LIMITS OF MULCHING, SEEDING, AND FERTILIZATION SHALL INCLUDE ALL DISTURBED AREAS WITHIN THE LIMITS OF CONSTRUCTION.
3. A BITUMINOUS TACK COAT LAYER (0.10 GAL/SQ.YD.) SHALL BE APPLIED TO THE EXISTING CONCRETE PAVEMENT (TAXIWAY C6) AND MILLED ASPHALT PAVEMENT PRIOR TO THE APPLICATION OF THE REQUIRED BITUMINOUS BINDER OVERLAY.
4. A BITUMINOUS TACK COAT LAYER (0.20 GAL/SQ.YD.) SHALL BE APPLIED TO THE EXISTING CONTINUOUSLY REINFORCED CONCRETE PAVEMENT (TAXIWAY C6) PRIOR TO THE PLACEMENT OF THE NON-WOVEN GEOTEXTILE FABRIC.
5. A BITUMINOUS TACK COAT LAYER (0.05 GAL/SQ.YD.) SHALL BE APPLIED BETWEEN EACH VERTICAL LIFT OF THE REQ'D 2 IN. (MIN.) ASPHALT OVERLAY.
6. THE EXISTING SOIL CEMENT BASE COURSE SHALL BE REMOVED AND REPLACED WITH A 5 IN. ECONOCRETE (P0306) BASE COURSE SHOULD IT BE DISTURBED DURING THE REMOVAL OF EXISTING CONCRETE PANELS OR FULL DEPTH PATCH REPAIRS. (AS SHOWN), COST TO BE ABSORBED.
7. FULL-DEPTH ASPHALT SHOULDER SHALL BE CONSTRUCTED AS INDICATED. SEE TYPICAL SECTION DETAIL.
8. A BITUMINOUS TACK COAT LAYER (0.5 GAL/SQ.YD.) SHALL BE APPLIED ATOP THE BITUMINOUS BASE COURSE OF THE FULL-DEPTH ASPHALT SHOULDER PRIOR TO THE APPLICATION OF THE BITUMINOUS BINDER OVERLAY.
9. FOR ALL AREAS WHERE THE SUBGRADE SOILS ARE EXPOSED DURING CONSTRUCTION, THEY SHALL BE COMPACTED TO 95% (MIN.) MODIFIED PROCTOR MAXIMUM DRY DENSITY (TYPICAL).
10. TOPSOIL SHALL BE TAKEN FROM THE MATERIAL EXCAVATED FROM THE EXISTING EARTH SHOULD AND USED TO TIE TO EXISTING GRADE. (AS SHOWN)



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TYPICAL TAXIWAY OVERLAY SECTION
REHABILITATE TAXIWAY C6
JACKSON MEDGAR EVERS INTERNATIONAL AIRPORT
JACKSON MUNICIPAL AIRPORT AUTHORITY
JACKSON, MISSISSIPPI

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SCALE:

PROJECT NO.
J.M.A.A. PROJECT NO.: 013-22
A.I.P. NO.:

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